

DIMENSION REFERENCE TABLE (PROWAG R302 - R305)				
	ITEM	MAX. RUNNING SLOPE	MAX. CROSS SLOPE	MIN. DIMENSIONS
(S)	SIDEWALK	RD GRADE (IN ROW) 5.0% (OUT OF ROW)	2.0%	4 FT WIDE (i)
(T)	TURNING SPACE	2.0%	2.0% (iv), (v)	4 FT X 4 FT (ii), (viii)
(R)	RAMP	8.3% (iii)	2.0% (iv), (v)	4 FT WIDE (viii)
(B)	BLENDED TRANSITION	5.0%	2.0% (iv), (v)	4 FT WIDE (viii)
(C)	CLEAR SPACE	5.0% (vi)	2.0% (iv), (v)	4 FT X 4 FT
	CROSSWALK	5.0%	2.0% (iv), (v)	10 FT WIDE
(F1)	FLARE WITHIN SIDEWALK	10.0% (vii)	-	-
(F2)	FLARE NOT IN SIDEWALK	25.0% (vii)	-	-

- (i) SIDEWALK WIDTH IS EXCLUSIVE OF THE WIDTH OF THE CURB. PROVIDE 5 FT X 5 FT PASSING SPACES AT 200 FT MAX. INTERVALS WHERE SIDEWALK WIDTH IS LESS THAN 5 FT (PROWAG R302.3 - R302.4). DRIVEWAYS MAY BE USED AS A PASSING SPACE WHERE A MINIMUM 5 FT WIDE AREA MEETING SIDEWALK REQUIREMENTS IS PROVIDED.
- (ii) USE A 5 FT MIN. DEPTH IN THE DIRECTION OF THE RAMP RUN FOR PERPENDICULAR RAMPS WHERE TURNING SPACE IS CONSTRAINED AT THE BACK OF SIDEWALK. (PROWAG R304.2.1)
USE A 5 FT MIN. WIDTH IN THE DIRECTION OF THE STREET CROSSING FOR PARALLEL RAMPS WHERE TURNING SPACE IS CONSTRAINED ON 2 OR MORE SIDES (PROWAG R304.3.1)
TURNING SPACE IS CONSTRAINED BY ANY VERTICAL DIFFERENCE GREATER THAN 3.0 IN.
- (iii) LENGTH OF 8.3% MAX RUNNING SLOPE IS NOT REQUIRED TO EXCEED 15 FT. STEEPEN GRADE TO MATCH EXISTING WITHIN AT LEAST 15 FT OR THE NEXT NEAREST JOINT IF THE MAX RUNNING SLOPE CAN NOT BE MET IN 15 FT. (PROWAG R304.2.2, R304.3.2).
- (iv) DO NOT EXCEED 5.0% CROSS SLOPE FOR CROSSINGS WITHOUT YIELD OR STOP CONTROL WHERE THERE IS NO STOP OR YIELD SIGN OR WHERE THERE IS A TRAFFIC SIGNAL DESIGNED FOR A GREEN PHASE.
DO NOT EXCEED 2.0% CROSS SLOPE FOR CROSSINGS WITH YIELD OR STOP CONTROL. (PROWAG R302.6)
- (v) MATCH THE HIGHWAY GRADE AT MIDBLOCK CROSSINGS (PROWAG R302.6.2)
- (vi) FLATTEN GUTTER SLOPE TO 5.0 PERCENT MAX AND MAINTAIN CLEAR SPACE RUNNING SLOPE ACROSS ENTIRE CURB CUT. (PROWAG R304.5.4) (SEE STD DWG PA 3, DETAIL A)
- (vii) MEASURE FLARE SLOPE PARALLEL TO CURB LINE. FLARES NOT IN THE SIDEWALK ARE PERMITTED TO BE A VERTICAL CURB FACE WHEN ACCESS FROM FLOWS IS NOT EXPECTED.
- (viii) MATCH TURNING SPACE, RAMP, AND BLENDED TRANSITION WIDTH TO CURB CUT AT PROJECTED BACK OF CURB.

DEFINITIONS

SINGLE DIAGONAL (APEX) RAMP: A CURB RAMP THAT SERVES BOTH PEDESTRIAN STREET CROSSINGS (PROWAG R207.2)

BLENDED TRANSITION: A RAISED PEDESTRIAN STREET CROSSING, DEPRESSED CORNER, OR SIMILAR CONNECTION BETWEEN THE PEDESTRIAN ACCESS ROUTE AT THE LEVEL OF THE SIDEWALK AND THE LEVEL OF THE PEDESTRIAN STREET CROSSING THAT HAS A GRADE OF 5 PERCENT OR LESS (PROWAG R105.5)

CLEAR SPACE: A SPACE BEYOND THE BOTTOM GRADE BREAK OF THE RAMP, PROVIDED WITHIN THE WIDTH OF THE PEDESTRIAN STREET CROSSING AND WHOLLY OUTSIDE THE PARALLEL VEHICLE TRAVEL LANE (PROWAG R304.5.5)

COMBINATION RAMP: RAMPS THAT PROVIDE A COMBINATION OF CONFORMING PARALLEL AND PERPENDICULAR RAMPS

CROSS SLOPE: THE GRADE THAT IS PERPENDICULAR TO THE DIRECTION OF PEDESTRIAN TRAVEL (PROWAG R105.5)

CURB CUT: THE AREA WHERE THE CURB IS CUT FLUSH TO THE FLOWLINE OF THE GUTTER OR ADJACENT ROADWAY EXCLUDING SLOPED CURB AT THE SIDES.

EXISTING PHYSICAL CONSTRAINTS: INCLUDE, BUT ARE NOT LIMITED TO, UNDERLYING TERRAIN, RIGHT-OF-WAY AVAILABILITY, UNDERGROUND STRUCTURES, ADJACENT DEVELOPED FACILITIES, DRAINAGE, OR THE PRESENCE OF A NOTABLE NATURAL OR HISTORIC FEATURE. (PROWAG R202.3.1)

GRADE BREAK: THE LINE WHERE TWO SURFACE PLANES WITH DIFFERENT GRADES MEET.

FLARE: A SLOPED SURFACE THAT PROVIDES A GRADED TRANSITION ON THE SIDE OF A CURB RAMP OR BLENDED TRANSITION BETWEEN THE CURB AND THE TURNING SPACE, MEASURED PARALLEL TO THE CURB.

PARALLEL RAMP: CURB RAMPS THAT HAVE A RUNNING SLOPE THAT IS IN-LINE WITH THE DIRECTION OF SIDEWALK TRAVEL AND LOWER THE SIDEWALK TO A LEVEL TURNING SPACE WHERE A TURN IS MADE TO ENTER THE PEDESTRIAN STREET CROSSING. (PROWAG ADVISORY R304.1)

PEDESTRIAN ACCESS ROUTE: A CONTINUOUS AND UNOBSTRUCTED PATH OF TRAVEL PROVIDED FOR PEDESTRIANS WITH DISABILITIES WITHIN OR COINCIDING WITH A PEDESTRIAN CIRCULATION PATH (PROWAG R105.5)

PEDESTRIAN CIRCULATION PATH: A PREPARED EXTERIOR OR INTERIOR SURFACE PROVIDED FOR PEDESTRIAN TRAVEL IN THE PUBLIC RIGHT-OF-WAY. (PROWAG R105.5)

PERPENDICULAR RAMP: CURB RAMPS THAT HAVE A RUNNING SLOPE THAT CUTS THROUGH OR IS BUILT UP TO THE CURB AT RIGHT ANGLES OR MEETS THE GUTTER BREAK AT RIGHT ANGLES WHERE THE CURB IS CURVED. (PROWAG ADVISORY R304.1)

PROWAG: "PROPOSED ACCESSIBILITY GUIDELINES FOR PEDESTRIAN FACILITIES IN THE PUBLIC RIGHT-OF-WAY". THE DOCUMENT USED AS THE PRIMARY RESOURCE FOR INFORMATION SHOWN (SEE NOTE 1).

RUNNING SLOPE: THE GRADE THAT IS PARALLEL TO THE DIRECTION OF PEDESTRIAN TRAVEL (PROWAG R105.5)

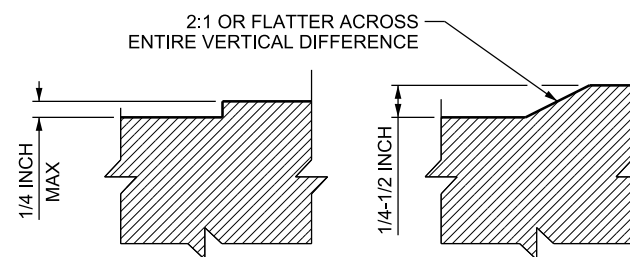
TURNING SPACE: SPACE AT THE TOP AND BOTTOM OF A RAMP TO ALLOW FOR DIRECTIONAL CHANGE, PERMITTED TO OVERLAP OTHER TURNING SPACES AND CLEAR SPACES (PROWAG R304.2.1, R304.3.1)

DESIGN NOTES APPLICABLE TO PA SERIES:

- A. CONVENTIONAL INDUSTRY TOLERANCES DO NOT APPLY TO DESIGN WORK. (PROWAG ADVISORY R103.1)
- B. PROVIDE A NO PEDESTRIAN (R9-3) SIGN AND A USE CROSSWALK (R9-3BP) PLAQUE (L OR R) IF PEDESTRIAN CROSSING IS PROHIBITED AT UNMARKED CROSSWALKS. (PROWAG ADVISORY R206) (SEE STD DWG PA 3 DETAIL B)
- C. A SINGLE DIAGONAL CORNER (APEX) RAMP IS PERMITTED TO SERVE BOTH PEDESTRIAN CROSSINGS IN SITUATIONS WHERE EXISTING PHYSICAL CONSTRAINTS PREVENT THE USE OF A CURB RAMP, BLENDED TRANSITION, OR A COMBINATION OF CURB RAMPS AND BLENDED TRANSITIONS. (PROWAG R207.2) USE OF A SINGLE DIAGONAL RAMP IS DISCOURAGED AND IS ONLY TO BE USED AT THE APPROVAL OF THE REGION TRAFFIC ENGINEER.
- D. ONLY A (4.0 FT) PORTION OF THE SIDEWALK IS REQUIRED TO COMPLY WITH THE REQUIREMENTS LISTED HEREIN WHERE SIDEWALKS ARE WIDER THAN 4.0 FT. (PROWAG ADVISORY R302.3)
- E. PROVIDE A FIRM, STABLE, AND SLIP RESISTANT SURFACE FOR ALL PEDESTRIAN ACCESS ROUTES (PROWAG R302.7)

GENERAL NOTES APPLICABLE TO PA SERIES:

- 1. INFORMATION PROVIDED IS BY REFERENCE FROM THE UNITED STATES ACCESS BOARD DOCUMENT "PROPOSED ACCESSIBILITY GUIDELINES FOR PEDESTRIAN FACILITIES IN THE PUBLIC RIGHT-OF-WAY" (PROWAG).
- 2. THE USE OF ALTERNATIVE DESIGNS, PRODUCTS, OR TECHNOLOGIES THAT MEET OR EXCEED THE REQUIREMENTS SHOWN IS PERMITTED. (PROWAG R102)
- 3. PROVIDE AN ALTERNATE PEDESTRIAN ACCESS ROUTE COMPLYING WITH SECTIONS 6D.01, 6D.02, AND 6G.05 OF THE MUTCD WHEN A PEDESTRIAN CIRCULATION PATH IS TEMPORARILY CLOSED. COMPLY WITH SECTIONS 6F.63, 6F.68, AND 6F.71 OF THE MUTCD WHERE PEDESTRIAN BARRICADES AND CHANNELIZING DEVICES ARE PROVIDED. (PROWAG R205)
- 4. CONNECT THE PEDESTRIAN ACCESS ROUTES AT EACH PEDESTRIAN STREET CROSSING WITH A CURB RAMP, BLENDED TRANSITION, OR A COMBINATION OF CURB RAMPS AND BLENDED TRANSITIONS. (PROWAG R207.1)
- 5. LOCATE THE CURB RAMP (EXCLUDING FLARED SIDES) OR BLENDED TRANSITION COMPLETELY WITHIN THE CROSSWALK (PROWAG R207.1)
- 6. CONSTRUCT ALL GRADE BREAKS TO BE FLUSH (PROWAG R302.7.1) PREVENT OR CORRECT ANY VERTICAL DIFFERENCES IN SURFACES GREATER THAN 0.5 IN. BEVEL VERTICAL DIFFERENCES BETWEEN 0.25 IN AND 0.5 IN WITH A SLOPE NOT STEEPER THAN 2H:1V ACROSS THE ENTIRE VERTICAL DIFFERENCE. (PROWAG R302.7.2) (SEE DETAIL BELOW)
- 7. CONSTRUCT GRADE BREAKS AT THE TOP AND BOTTOM OF CURB RAMP RUNS PERPENDICULAR TO THE DIRECTION OF THE RAMP RUN. DO NOT PLACE GRADE BREAKS ON THE SURFACES OF RAMP RUNS AND TURNING SPACES. (PROWAG R304.5.2)
- 8. SITE CONDITIONS MAY VARY. CONFIGURATION OF RAMP, BLENDED TRANSITION, TURNING SPACE, AND CLEAR SPACE MAY BE ALTERED, BUT THEY MUST MEET DIMENSIONS AND SLOPES SHOWN HERE. THE USE OF ITEMS SUCH AS FLARES AND CURBWALLS ARE AT THE DISCRETION OF THE ENGINEER.
- 9. DO NOT USE DIRECTIONAL ACCESSES WHEN PEDESTRIANS CAN APPROACH THE DETECTABLE WARNING SURFACE FROM MORE THAN ONE DIRECTION.
- 10. CONSTRUCT ALL PEDESTRIAN ACCESS ROUTE ELEMENTS SO MAXIMUM OR MINIMUM VALUES ARE NOT EXCEEDED.
- 11. PROVIDE TOOLED JOINTS ON ALL GRADE BREAKS.



VERTICAL SURFACE DISCONTINUITIES
SEE NOTE 6

REVISIONS

NO.	DATE	APPR.	REMARKS
1	2/22/18	CGW	NEW DRAWING

UTAH DEPARTMENT OF TRANSPORTATION
STANDARD DRAWINGS FOR ROAD AND BRIDGE CONSTRUCTION
SALT LAKE CITY, UTAH

RECOMMENDED FOR APPROVAL
Randy J. Park
CHAIRMAN STANDARDS COMMITTEE
APPROVED
DEPUTY DIRECTOR

DATE: FEB. 22, 2018
DATE: FEB. 22, 2018

GENERAL PEDESTRIAN ACCESS

STANDARD DRAWING TITLE

STD. DWG. NO.

PA 1

SUPPLEMENTAL DRAWING