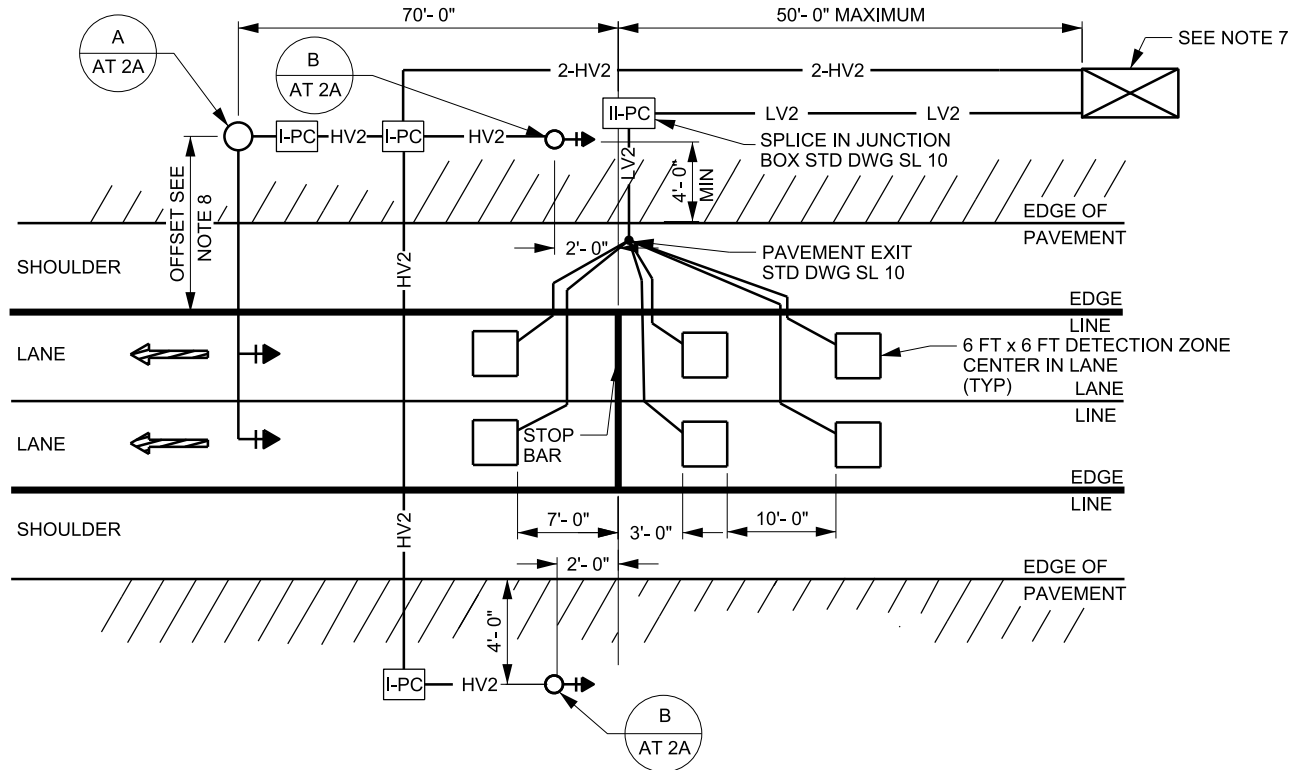
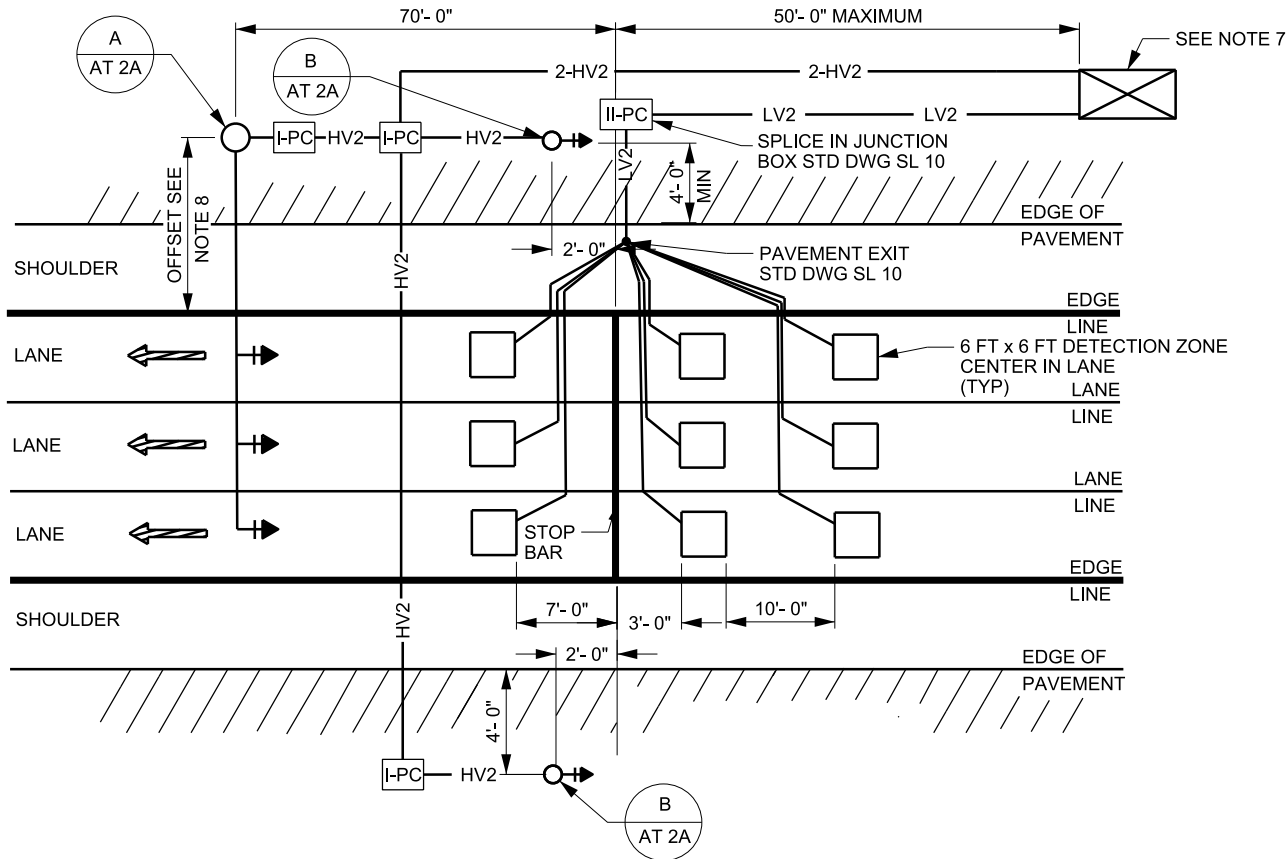


**A** ONE LANE RAMP METER  
DETECTION ZONE LAYOUT DETAIL



**B** TWO LANE RAMP METER  
DETECTION ZONE LAYOUT DETAIL



**C** THREE LANE RAMP METER  
DETECTION ZONE LAYOUT DETAIL

**NOTES:**

1. USE DETECTION TECHNOLOGY PER THE PLANS. IF USING RADAR OR MAGNETOMETERS, ADJUST DETECTION ZONE PER THE MANUFACTURER'S RECOMMENDATIONS TO EMULATE 6 X 6 FT LOOPS.
2. PLACE JUNCTION BOXES ACCORDING TO STD DWG AT 7A. REFER TO STD DWG SL 10 FOR DETECTION.
3. USE PREFORMED LOOPS IN NEW PAVEMENT. SAW CUT LOOPS ARE NOT ALLOWED.
4. USE SEPARATE CONDUCTOR HOME RUN TO CABINET FOR EACH LOOP.
5. TAG EACH LOOP WIRE IN EACH JUNCTION BOX, BEGINNING WITH FIRST LOOP IN LANE CLOSEST TO SHOULDER.
6. SEE STD DWG AT 5C FOR DETECTOR NUMBERING SCHEME.
7. USE THE CURRENT EDITION OF AASHTO ROADSIDE DESIGN GUIDE AND STD DWG DD 18 FOR CLEAR ZONE REQUIREMENTS. CLEAR ZONE MAY EXTEND INTO CUT OR FILL SLOPES.
8. PLACE RAMP METER MAST ARM ASSEMBLY AND CABINET A MINIMUM OF 1.2 TIMES THE AASHTO CLEAR ZONE DISTANCE AWAY FROM THE TRAVELED WAY, OR SHIELD AS PER THE CURRENT EDITION OF THE AASHTO ROADSIDE DESIGN GUIDE.
9. PLACE CABINET ON RIGHT SIDE OF TRAFFIC UNLESS SPACE IS UNAVAILABLE. CABINET ON LEFT SIDE MUST MEET CLEAR ZONE REQUIREMENTS FOR MAINLINE AND RAMP.

SUPPLEMENTAL DRAWING

REVISIONS	
NO.	DATE
1	02/28/13

UTAH DEPARTMENT OF TRANSPORTATION  
STANDARD DRAWINGS FOR ROAD AND BRIDGE CONSTRUCTION  
SALT LAKE CITY, UTAH

RECOMMENDED FOR APPROVAL  
*Carl D. ...*  
CHAIRMAN STANDARDS COMMITTEE

DATE  
FEB. 28, 2013

DATE  
FEB. 28, 2013

DEPUTY DIRECTOR

RAMP METER  
DETECTION LAYOUT

STANDARD DRAWING TITLE

STD. DWG. NO.  
AT 5A